



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
16 STATE HOUSE STATION
AUGUSTA, MAINE
04333-0016

JOHN ELIAS BALDACCI
GOVERNOR

DAVID A. COLE
COMMISSIONER

March 30, 2007
Subject: **Medway**
Project No. BR-1008(600)X
Pin No. 10086.00
Amendment No. 2

Dear Sir/Ms:

Please make the following changes to the Bid Documents.

In the Bid Book, REMOVE in its entirety the existing "Schedule of Items" ten pages total dated 070226 and REPLACE with the attached updated "Schedule of Items" ten pages total dated 070329.

REMOVE the "Heavy Wage Rates" three pages total and REPLACE with the attached new "Highway Wage Rates" three pages total.

ADD the attached "Special Provision, Section 502, Structural Concrete (Bridge Drains)" six pages total dated March 23, 2007.

REMOVE in its entirety the existing "Special Provision, Section 510, Special Detours" three pages total dated December 28, 2006 and REPLACE with the attached updated "Special Provision Section 510 Special Detours" three pages total dated March 23, 2007.

REMOVE in its entirety the existing "Special Provision, Section 652, Maintenance of Traffic" four pages total dated February 22, 2007, and REPLACE with the attached updated "Special Provision 652 Maintenance of Traffic" six pages total dated March 23, 2007.

REMOVE "Special Provision, Section 652, Maintenance of Traffic (Traffic Control)" one page total dated January 19, 2007.



PRINTED ON RECYCLED PAPER

On Plan Sheet 2 of 53 titled: "Estimated Quantities" under Estimated Quantities, within Item No. "502.249 STR CONC PIERS (PLACED UNDER WATER)" DELETE the existing quantity and unit: "(185 m3)-1-LS " and REPLACE it with the following: "(185 m3)". Make this change in pen and ink.

In the Plan Sheets, on Plan Sheet 2 of 53 titled "Estimated Quantities" under Estimated Quantities, within Item No. "504.702 STR STEEL FAB & DEL, WELDED" DELETE the existing quantity: "(227,900 kg)" and REPLACE it with the following: "(290,000 kg)". Make this change in pen and ink.

On Plan Sheet 2 of 53 titled: "Estimated Quantities" under Estimated Quantities, within Item No. "504.71 STR STEEL ERECTION" DELETE the existing quantity: "(227,900 kg)" and REPLACE it with the following: "(290,000 kg)". Make this change in pen and ink.

On Plan Sheet 2 of 53 titled: "Estimated Quantities" under General Construction Notes, DELETE in its entirety the Note 15 that begins: "All substructures units shall have... ". Make this change in pen and ink.

On Plan Sheet 28 of 53 titled: "Pier Plans & Rebar" under Seal Cofferdam Notes, ADD Note 6 that states: "Payment for removal of material within the cofferdam shall be incidental to the cofferdam item." Make this change in pen and ink.

On Plan Sheet 32 of 53 titled: "Girder Details" under Structural Steel Notes, DELETE in its entirety the existing Note 8 that begins: "At locations marked with..." and REPLACE with the following new Note 8 that states: "At locations marked with an asterisk (*), the designated diaphragms shall be changed to a Type MD diaphragm as required to accommodate the Contractor's deck placement sequence. No extra compensation will be allowed for any diaphragms so substituted, and any additional costs will be considered incidental to the Contract Items." Make this change in pen and ink.

On Plan Sheet 33 of 53 titled: "Framing Plan" under detail Framing Plan, ADD asterisks to all diaphragms designations nearest stations 1+129.46, 1+155.66, 1+176.66, and 1+202.86. Make this change in pen and ink.

On Plan Sheet 38 of 53 titled: ""Superstructure Plan 1" under Superstructure Notes, ADD Note 13 that states: "If the superstructure slab concrete is placed in one continuous operation, the concrete shall be kept plastic one complete span behind the span being placed." Make this change in pen and ink.

On Plan Sheet 38 of 53 titled: "Superstructure Plan 1" under Superstructure Notes, ADD Note 14. that states: "If the deck is not placed in one continuous operation, a minimum of 5 days shall elapse between successive partial placements. Concrete in a placement shall be kept plastic one complete span behind the span being placed. No more than 2 placements shall be used to place the entire deck." Make this change in pen and ink.

On Plan Sheet 41 of 53 titled: "Superstructure Reinforcing 1" in the Superstructure Reinforcing Steel Schedule, ADD the following to the bent bars schedule: MARK=S1655, QTY=88, LENGTH=2625, TYPE=SJ, A=570, B=250, C=430, D=275, E=1100, LOCATION=End of Slab. Make this change in pen and ink.

On Plan Sheet 44 of 53 titled: "Superstructure Details" within detail Bridge Cross Section Through Transition Barrier, DELETE any reference to bar mark MC1950. Make this change in pen and ink.

On Plan Sheet 45 of 53 titled: "'BR27' Rail Details" within detail Typical Section, DELETE the note "TS 102x76x4.8" in its entirety. Make this change in pen and ink.

On Plan Sheet 45 of 53 titled: "'BR27' Rail Details" within detail Typical Elevation BR27D Railing, DELETE the existing three "TS 101.6x76.2x7.40" notes, and REPLACE them with the following updated notes: "TS 102x76x6.4." Make this change in pen and ink.

The following questions have been received.

Question: In regards to your answer to question no. 2 on RFI #5 dated 3/21/07, if the protective coating of the entire substructure (note 15) is not incidental, where is the protective coating for the entire structure paid? We have a quantity of 1000 m² for the protective coating areas listed in note 14 and 740 m² for the entire substructures. Note 14 does not list the entire substructure as areas to be coated.

Response: See change earlier in this amendment deleting note 15.

Question: Could you please specify what grade the anchor bolts are required to be? F1554 comes in Grade 36, 55, 105.

Response: The anchor bolts shall be Grade 105.

Question: Note #7 on sheet 36 states that the masonry plate is to be galvanized. Is there any coating required on the beveled sole plate?

Response: Per note 7, which refers to note 22 on sheet 2 of 53 added in amendment #1, the sole plate shall be coated with a zinc rich coating system.

Question: Savage Street is now only 18' wide. Is that going to be sufficient width for detoured traffic and people who live on the street? Or, do we need to include widening the street to 7.2 m?

Response: Savage Street shall be paved for a minimum width of 6 meters. The detour was deemed adequate by the Department's traffic section.

Question: On Birch Street where the DOT is asking for PMRAP, can we use gravel and hot top instead? Can it stay on Birch Street Ext. or does it have to be removed?

Response: Gravel and hot top may be used in locations where a minimum of 75 mm PMRAP is called for. The wearing surface shall remain upon the completion of the project on Birch St. Ext.

Question: In that ledge elevation is undefined, shouldn't 502.249 "St. Concrete Piers Placed Underwater" be paid for by the cubic meter?

Response: See change earlier in this amendment to item no. 502.249.

Question: How is "Structural Earth Excavation Piers" paid?

Response: See change earlier in this amendment to plan sheet 28 adding note 6.

Question: We request additional wage classifications for bridge construction tasks.

Response: The wage rates have been changed from heavy to highway construction type earlier in this amendment. Additional wage rates are included in this change. Other wage rates may be requested after award. It will take two to three week to obtain the rates.

Question: I asked the question in my RFI of 3/19/07 re: the "Damages per Violation" in section 652. This still has not been addressed.

Response: See change earlier in this amendment removing the spec.

Question: Sheet 45 shows typical railing elevation with being 7.4 thick. Typical section shows 4.8 thick. Which is correct?

Response: See change earlier in this amendment.

Question: If rail post tube is 4.8 thick, is 11 mm fillet weld necessary as shown on base weld detail?

Response: Yes, a 11 mm fillet weld is required for the post to plate connection as shown on the base weld detail.

Question: Sheet 2 has a Structural steel Quantity of 227,900 kg. We come up with a weight of 291,276+/- kg.

Response: The steel weight as modified in Amendment 1. If the actual quantity is different than the estimated quantity on Lump Sum items, then Note 21 on sheet 2 that begins: "Quantities included for items measured..." shall apply.

Question: Are the bars in the end post on sheet 46 MMFX2? If yes, are in BI #503.24 or BI #526.34?

Response: The bars in the end post are to be regular black.

Question: Are the curb & sidewalk bars (M) on sheet 47 & curb & light base bars (M) on sheet 41 to be included in BI #502.49 or BI #503.24?

Response: The bars shown in the reinforcing schedule on Sheet 41 shall be paid for under Item 502.49. All other "M" bars shall be paid for under Item 503.24.

Question: Sheet 44 calls out a bar mark MC 1950. What sheet is the reinforcing schedule for this bar on?

Response: See change earlier in this amendment removing MC1950. The reinforcing steel within the end posts are given in the Rail Endpost Details.

Question: Please Clarify where flaggers and Portable Changeable Message Signs will be paid. There is a paid item for both of these items and both of these items are included in item 652.39 Work Zone Traffic Control.

Response: Flaggers are to be paid for under Item 652.38 Flagger. The Portable Changeable Message Signs shall be paid for under Item 652.41.

Question: Since it is the Contractor's responsibility to maintain a smooth travel surface on the detour, can the detour be paved with Hot Mix Asphalt?

Response: Yes, the detour may be paved with Hot Mix Asphalt.

Question: At the Contractor's option, can the gravel and pavement for the detour on Libby Road be left in place upon completion?

Response: The gravel and pavement may be left in place in Libby Road upon the completion of the project.

Consider this information and changes prior to submitting your bid on April 4, 2007.

Sincerely,

A handwritten signature in black ink, appearing to read "S. Bickford" with a stylized flourish at the end.

Scott Bickford

Contracts & Specifications Engineer

SCHEDULE OF ITEMS

CONTRACT ID: 010086.00 & 10086.10 PROJECT:BR-1008(600)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE		BID AMOUNT	
			DOLLARS	CTS	DOLLARS	CTS

SECTION 0001 GENERAL ITEMS

0010	202.19 REMOVING EXISTING BRIDGE	LUMP		LUMP			
0020	203.20 COMMON EXCAVATION	M3	1000.000				
0030	203.21 ROCK EXCAVATION	M3	25.000				
0040	203.24 COMMON BORROW	M3	3200.000				
0050	203.25 GRANULAR BORROW	M3	530.000				
0060	206.082 STRUCTURAL EARTH EXCAVATION - MAJOR STRUCTURES	M3	230.000				
0070	304.10 AGGREGATE SUBBASE COURSE - GRAVEL	M3	1550.000				
0080	403.209 HOT MIX ASPHALT 9.5 MM (SIDEWALKS, DRIVES, INCIDENTALS)	MG	15.000				
0090	403.210 HOT MIX ASPHALT 9.5 MM NOMINAL MAX SIZE	MG	450.000				
0100	403.213 HOT MIX ASPHALT 12.5 MM, BASE	MG	315.000				

SCHEDULE OF ITEMS

CONTRACT ID: 010086.00 & 10086.10 PROJECT:BR-1008(600)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE		BID AMOUNT	
			DOLLARS	CTS	DOLLARS	CTS
0110	409.15 BITUMINOUS TACK COAT APPLIED	740.000 L				
0120	502.219 STRUCTURAL CONCRETE, ABUTMENTS AND RETAINING WALLS	LUMP	LUMP			
0130	502.239 STRUCTURAL CONCRETE PIERS	LUMP	LUMP			
0140	502.24 STRUCTURAL CONCRETE PIERS (PLACED UNDER WATER)	185.000 M3				
0150	502.26 STRUCTURAL CONCRETE ROADWAY AND SIDEWALK SLABS ON STEEL BRIDGES	LUMP	LUMP			
0160	502.31 STRUCTURAL CONCRETE APPROACH SLABS	LUMP	LUMP			
0170	502.49 STRUCTURAL CONCRETE CURBS AND SIDEWALKS	LUMP	LUMP			
0180	503.12 REINFORCING STEEL, FABRICATED AND DELIVERED	20200.000 KG				
0190	503.13 REINFORCING STEEL, PLACING	20200.000 KG				
0200	503.24 MMFX 2 REINFORCING STEEL, FABRICATED & DELIVERED	5750.000 KG				

MAINE DEPARTMENT OF TRANSPORTATION
SCHEDULE OF ITEMS

PAGE: 3
DATE: 070329
REVISED:

CONTRACT ID: 010086.00 & 10086.10 PROJECT:BR-1008(600)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE		BID AMOUNT	
			DOLLARS	CTS	DOLLARS	CTS
0210	503.25 MMFX 2 REINFORCING STEEL, PLACING	5750.000 KG				
0220	504.71 STRUCTURAL STEEL ERECTION	LUMP	LUMP			
0230	505.08 SHEAR CONNECTORS	LUMP	LUMP			
0240	507.0811 STEEL BRIDGE RAILING, 2 BAR	LUMP	LUMP			
0250	507.0821 STEEL BRIDGE RAILING, 3 BAR	LUMP	LUMP			
0260	508.14 HIGH PERFORMANCE WATERPROOFING MEMBRANE	LUMP	LUMP			
0270	510.10 SPECIAL DETOUR____(M) ROADWAY WIDTH VEHICULAR & PEDESTRIAN TRAFFIC NOT SEPARATED	LUMP	LUMP			
0280	511.07 COFFERDAM: EXISTING PIER NO. 1	LUMP	LUMP			
0290	511.07 COFFERDAM: NEW PIER NO. 1	LUMP	LUMP			
0300	511.07 COFFERDAM: NEW PIER NO. 2	LUMP	LUMP			

SCHEDULE OF ITEMS

CONTRACT ID: 010086.00 & 10086.10 PROJECT:BR-1008(600)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE		BID AMOUNT	
			DOLLARS	CTS	DOLLARS	CTS
0310	514.06 CURING BOX FOR CONCRETE CYLINDERS	1.000 EA				
0320	515.21 PROTECTIVE COATING FOR CONCRETE SURFACES	LUMP	LUMP			
0330	520.21 EXPANSION DEVICE - GLAND SEAL	1.000 EA				
0340	520.22 EXPANSION DEVICE - COMPRESSION SEAL	1.000 EA				
0350	523.52 BEARING INSTALLATION	20.000 EA				
0360	523.5401 LAMINATED ELASTOMERIC BEARINGS, FIXED	5.000 EA				
0370	523.5402 LAMINATED ELASTOMERIC BEARINGS, EXPANSION	15.000 EA				
0380	526.34 PERMANENT CONCRETE TRANSITION BARRIER	4.000 EA				
0390	534.30 PRECAST STRUCTURAL CONCRETE (TIE- BACKS AND DEADMEN)	LUMP	LUMP			
0400	603.17 450 MM CULVERT PIPE OPTION I	31.000 M				

SCHEDULE OF ITEMS

CONTRACT ID: 010086.00 & 10086.10 PROJECT:BR-1008(600)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE		BID AMOUNT	
			DOLLARS	CTS	DOLLARS	CTS
0410	605.09 150 MM UNDERDRAIN TYPE B	M 145.000				
0420	606.1721 BRIDGE TRANSITION - TYPE 1	EA 4.000				
0430	606.23 GUARDRAIL TYPE 3C - SINGLE RAIL	M 113.000				
0440	606.232 GUARDRAIL TYPE 3C - OVER 4.5 M RADIUS	M 8.000				
0450	606.265 TERMINAL END - SINGLE RAIL - GALVANIZED STEEL	EA 1.000				
0460	606.35 GUARDRAIL DELINEATOR POST	EA 2.000				
0470	606.353 REFLECTORIZED FLEXIBLE GUARDRAIL MARKER	EA 8.000				
0480	606.79 GUARDRAIL 350 FLARED TERMINAL	EA 3.000				
0490	609.31 CURB TYPE 3	M 50.000				
0500	610.08 PLAIN RIPRAP	M3 1150.000				
0510	613.319 EROSION CONTROL BLANKET	M2 55.000				

MAINE DEPARTMENT OF TRANSPORTATION
SCHEDULE OF ITEMS

PAGE: 6
DATE: 070329
REVISED:

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CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE		BID AMOUNT	
			DOLLARS	CTS	DOLLARS	CTS
0520	615.07 LOAM	80.000 M3				
0530	618.1401 SEEDING METHOD NUMBER 2 - PLAN QUANTITY	16.000 UN				
0540	619.1201 MULCH - PLAN QUANTITY	16.000 UN				
0550	619.1401 EROSION CONTROL MIX	155.000 M3				
0560	620.58 EROSION CONTROL GEOTEXTILE	1500.000 M2				
0570	626.21 METALLIC CONDUIT	20.000 M				
0580	626.22 NON-METALLIC CONDUIT	165.000 M				
0590	626.32 600 MM FOUNDATION	2.000 EA				
0600	626.35 CONTROLLER CABINET FOUNDATION	1.000 EA				
0610	627.711 WHITE OR YELLOW PAINTED PAVEMENT MARKING LINE (PLAN QUANTITY)	930.000 M				
0620	629.05 HAND LABOR, STRAIGHT TIME	40.000 HR				

SCHEDULE OF ITEMS

CONTRACT ID: 010086.00 & 10086.10 PROJECT:BR-1008(600)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE		BID AMOUNT	
			DOLLARS	CTS	DOLLARS	CTS
0630	631.121 HEAVY DUTY ALL PURPOSE EXCAVATOR (INCLUDING OPERATOR)	25.000 HR				
0640	631.15 ROLLER, EARTH AND BASE COURSE (INCLUDING OPERATOR)	30.000 HR				
0650	631.172 TRUCK - LARGE (INCLUDING OPERATOR)	25.000 HR				
0660	634.16 HIGHWAY LIGHTING	LUMP	LUMP			
0670	634.21 CONVENTIONAL LIGHT STANDARD	4.000 EA				
0680	635.14 PREFABRICATED CONCRETE MODULAR GRAVITY WALL	50.000 M2				
0690	637.071 DUST CONTROL	LUMP	LUMP			
0700	639.18 FIELD OFFICE TYPE A	1.000 EA				
0710	652.38 FLAGGER	300.000 HR				
0720	652.39 WORK ZONE TRAFFIC CONTROL	LUMP	LUMP			
0730	652.41 PORTABLE - CHANGEABLE MESSAGE SIGN	4.000 EA				

SCHEDULE OF ITEMS

CONTRACT ID: 010086.00 & 10086.10 PROJECT:BR-1008(600)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE		BID AMOUNT	
			DOLLARS	CTS	DOLLARS	CTS
0740	656.75 TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL	LUMP	LUMP			
0750	659.10 MOBILIZATION	LUMP	LUMP			
0760	660.21 ON-THE-JOB TRAINING (BID)	1000.000 HR				
	SECTION 0001 TOTAL					.

SECTION 0002 BRIDGE ITEMS

0770	504.702 STRUCTURAL STEEL FABRICATED AND DELIVERED, WELDED PIN 10086.10	LUMP	LUMP			
	SECTION 0002 TOTAL					.

SECTION 0003 NON-ROCK SOCKETED ALTERNATE 1
ALTERNATE 1
ALT GROUP AF1

0780	501.231 DYNAMIC LOADING TEST	2.000 EA				
0790	501.50 STEEL H-BEAM PILES 132 KG/M, DELIVERED	126.000 M				
0800	501.501 STEEL H-BEAM PILES 132 KG/M, IN PLACE	114.000 M				

MAINE DEPARTMENT OF TRANSPORTATION
SCHEDULE OF ITEMS

PAGE: 9
DATE: 070329
REVISED:

CONTRACT ID: 010086.00 & 10086.10 PROJECT:BR-1008(600)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE		BID AMOUNT	
			DOLLARS	CTS	DOLLARS	CTS
0810	501.90 PILE TIPS	EA 10.000				
0820	501.91 PILE SPLICES	EA 10.000				
0830	501.92 PILE DRIVING EQUIPMENT MOBILIZATION	LUMP	LUMP			
	SECTION 0003 TOTAL					.

SECTION 0004 ROCK SOCKETED ALTERNATE 2
ALTERNATE 2
ALT GROUP AF2

0840	501.38 STEEL H-BEAM PILES 62 KG/M, DELIVERED	M 30.000				
0850	501.382 ROCK-SOCKETED STEEL H-BEAM PILES 62 KG/M, IN-PLACE	M 30.000				
0860	501.70 STEEL PIPE PILES, DELIVERED	M 126.000				
0870	501.701 STEEL PIPE PILES, IN PLACE	M 114.000				
0880	501.90 PILE TIPS	EA 10.000				
0890	501.91 PILE SPLICES	EA 10.000				

MAINE DEPARTMENT OF TRANSPORTATION
SCHEDULE OF ITEMS

PAGE: 10
DATE: 070329
REVISED:

CONTRACT ID: 010086.00 & 10086.10 PROJECT:BR-1008(600)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE		BID AMOUNT	
			DOLLARS	CTS	DOLLARS	CTS
0900	501.92 PILE DRIVING EQUIPMENT MOBILIZATION	LUMP	LUMP			
	SECTION 0004 TOTAL					
	TOTAL BID					

the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (ii)).

In the listing above, the "SU" designation means that rates listed under the identifier do not reflect collectively bargained wage and fringe benefit rates. Other designations indicate unions whose rates have been determined to be prevailing.

WAGE DETERMINATION APPEALS PROCESS

1.) Has there been an initial decision in the matter? This can be:

- * an existing published wage determination
- * a survey underlying a wage determination
- * a Wage and Hour Division letter setting forth a position on a wage determination matter
- * a conformance (additional classification and rate) ruling

On survey related matters, initial contact, including requests for summaries of surveys, should be with the Wage and Hour Regional Office for the area in which the survey was conducted because those Regional Offices have responsibility for the Davis-Bacon survey program. If the response from this initial contact is not satisfactory, then the process described in 2.) and 3.) should be followed.

With regard to any other matter not yet ripe for the formal process described here, initial contact should be with the Branch of Construction Wage Determinations. Write to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2.) If the answer to the question in 1.) is yes, then an interested party (those affected by the action) can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Write to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and by any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3.) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative

Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

4.) All decisions by the Administrative Review Board are final.

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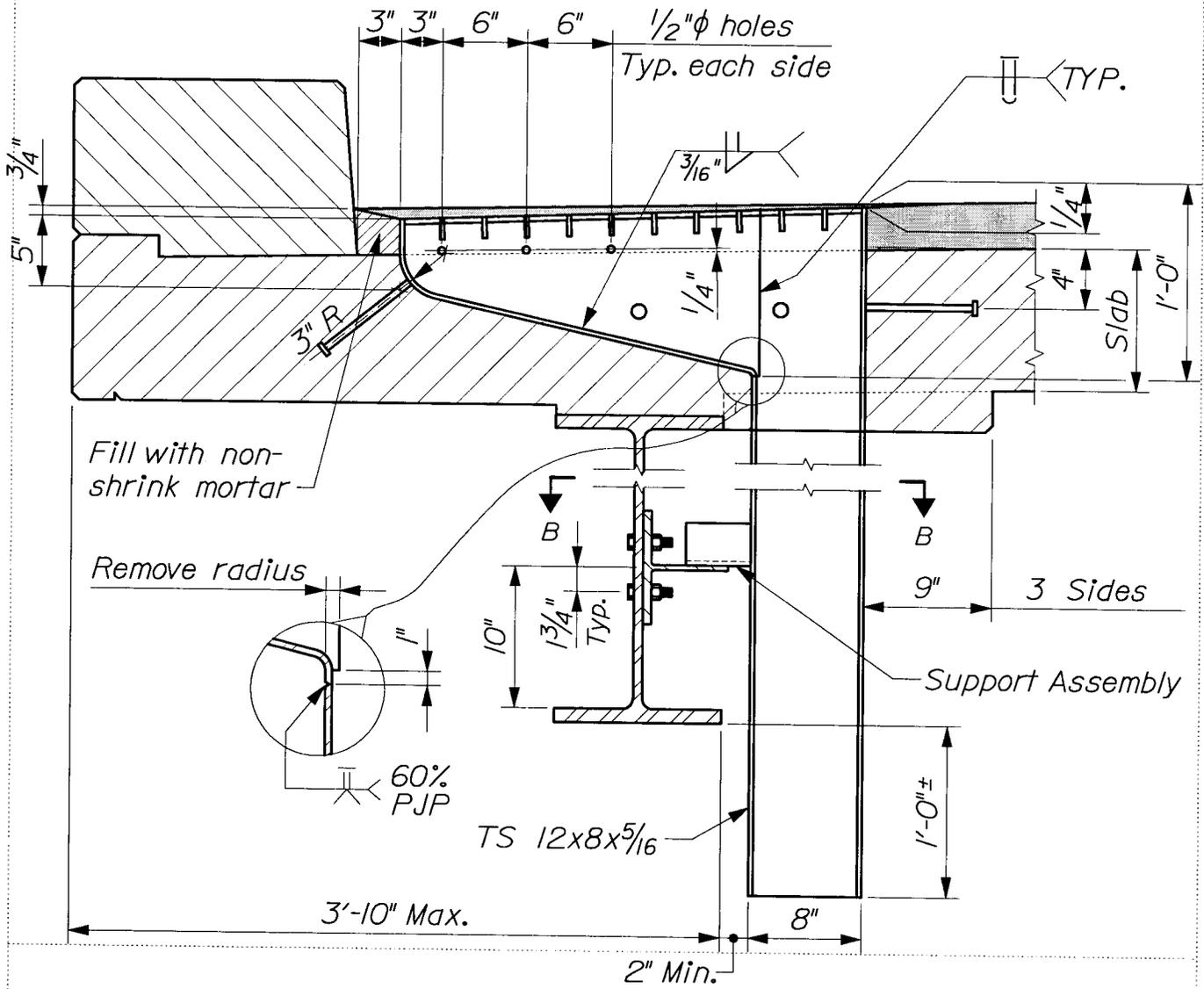
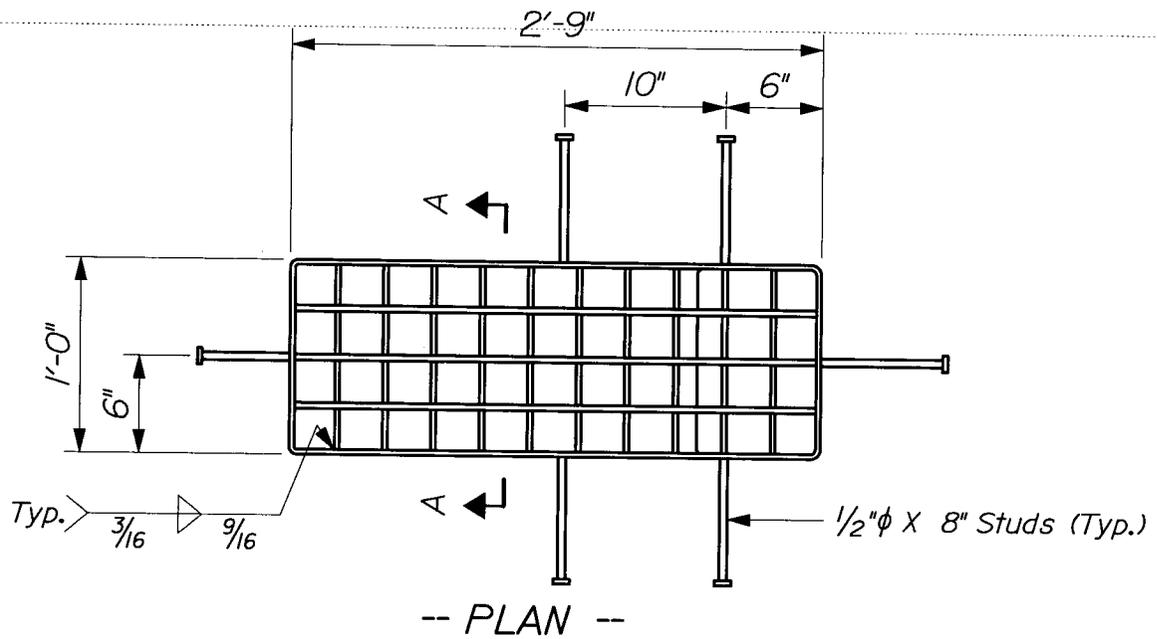
END OF GENERAL DECISION

Medway
BR 1008(600)X
March 23, 2007

SPECIAL PROVISION
SECTION 502
STRUCTURAL CONCRETE
(Bridge Drains)

The following shall be added to the 502.17, Bridge Drains and Incidental Drainage, of the 2002 Revision of the Standard Specifications:

The attached bridge drain drawings shall be used on the project.



-- SECTION --

Supplemental
Standard Detail

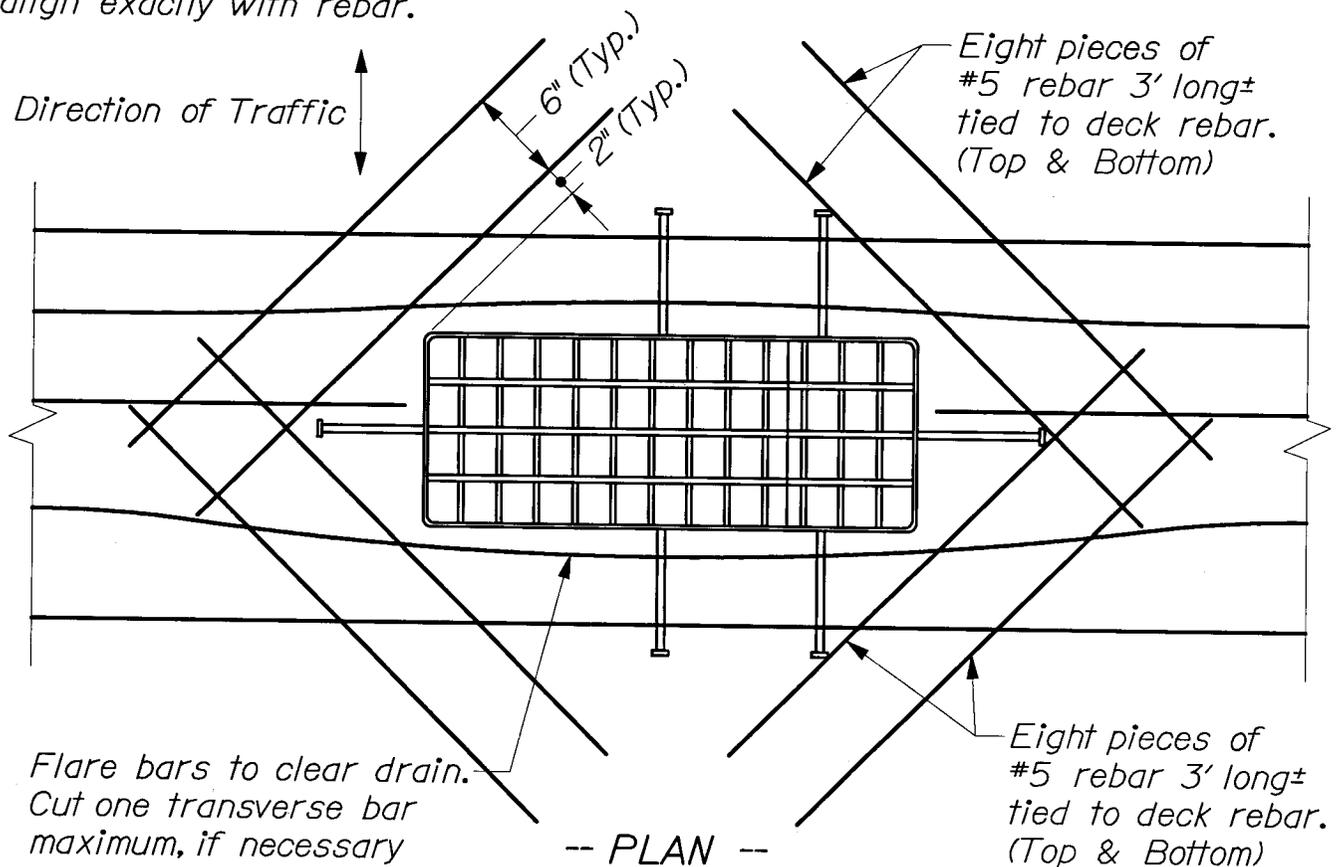
BRIDGE DRAIN TYPE A

502(05A)

Dec. 2004

Locate drain in center between 1st & 3rd rebar. Cut center rebar 2" from drain.

Holes for Girder attachment are pre-drilled. Drain may not align exactly with rebar.

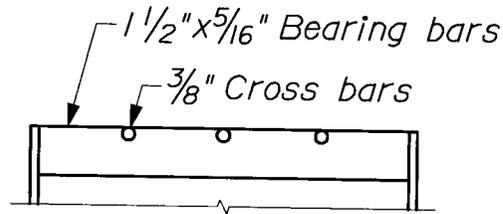


SLAB REINFORCING AT BRIDGE DRAINS

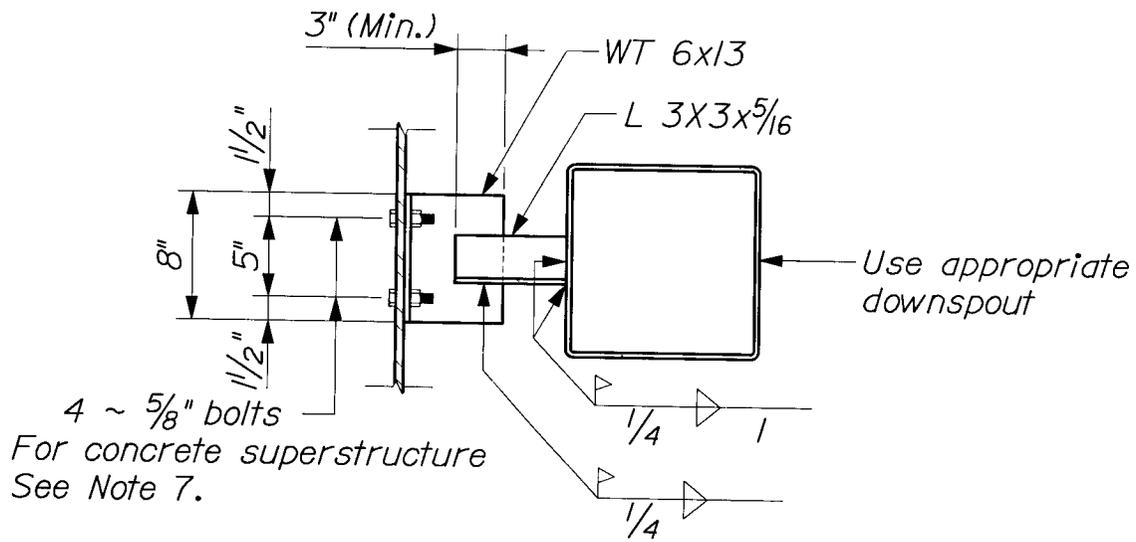
Supplemental
Standard Detail

502(06B)

Dec. 2004

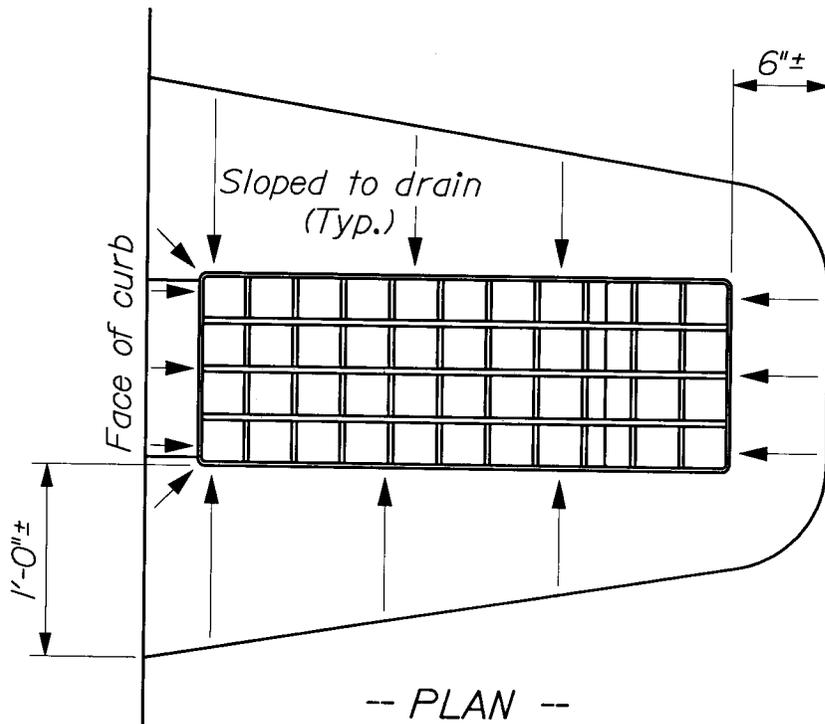


SECTION A-A



4 ~ 5/8" bolts
For concrete superstructure
See Note 7.

-- SECTION B-B --



PAVEMENT DEPRESSION
AT BRIDGE DRAINS

Supplemental
Standard Detail

502(06C)

Dec. 2004

NOTES:

1. All plates, if any, shall be $\frac{1}{4}$ inch thick and shall conform to ASTM A 36.
2. The downspout shall conform to ASTM A 500.
3. Grating shall be a commercial heavy - duty grating with $1\frac{1}{2}$ "x $\frac{5}{16}$ " bearing bars spaced at $2\frac{3}{8}$ " and $\frac{3}{8}$ " ϕ cross bars spaced at 4". Grating shall be centered in the drain top.
4. The $\frac{1}{2}$ " ϕ holes are not required when a concrete wearing surface is specified.
5. If the minimum thickness of concrete below the drain is two inches or less, the concrete haunch shall be extended as shown.
6. Shear connectors welded to top flange of beam may need to be adjusted should an interference with the bridge drain occur.
7. Drains and L $3 \times 3 \times \frac{5}{16}$ shall be blast cleaned to the requirements of SSPC-SP6/NACE 3 and hot-dipped galvanized in accordance with ASTM A 123. Steel beam attachment (WT 6x13 and associated fasteners) shall meet the same material specification and protective coating requirements as the structural steel.
8. Concrete superstructure support assembly configuration shall be similar to views shown utilizing concrete anchoring material listed on the Maine Department of Transportation Prequalified List. WT 6x13 and fastener hardware shall be galvanized in accordance with ASTM A 123 and A 153 or B 695, Class 50, Type 1.
9. Payment for bridge drains will be as specified under Subsection 502.19 of the Standard Specifications.
10. The additional reinforcing steel around each bridge drain will not be paid for directly. Payment will be considered incidental to contract items.

SPECIAL PROVISION
SECTION 510
SPECIAL DETOURS

Section 510, Special Detours of the Standard Specifications is amended as follows:

510.01 Description: Modify this entire subsection to read as follows:

This work shall consist of the design, construction, and removal of temporary access roads, ramps, and rehabilitation of existing roads required for the satisfactory maintenance of vehicular traffic. The access roads shall connect Route 116 to the Interstate.

Easements or right-of-way for the Special Detour structure and approaches will be furnished by the Department and will be shown on the contract plans. The detour shall be constructed within the obtained right of way.

510.03 Vehicular and Pedestrian Traffic Not Separated: Add the following:

An access road shall be designed and constructed to provide entrance only access from Libby Road to the Interstate Rest Area on the northbound side. A stop sign shall be placed at the end of the access road. The road shall be designed to handle legal Interstate loading. Provisions for drainage shall be made for the widened section. The access road shall be removed in its entirety at the completion of the project and the site returned to its original condition.

An access road shall be designed and constructed to provide exit only access from the southbound lane of the Interstate to Savage Street. A stop sign shall be placed at the end of the access road. The road shall be designed to handle legal Interstate loading. Provisions for drainage shall be made for the widened section. The access road shall be removed in its entirety at the completion of the project and the site returned to its original condition.

Savage Street shall have a 12.5 mm overlay placed prior to the opening the detour route to traffic. The road shall be paved for a minimum of 6 meters total width. An adequate turning radius shall be provided at the end of Savage Street onto Route 116. The drainage culvert at the end of Savage Street shall be replaced in kind prior to opening the detour route.

Libby Road shall be widened to the east to allow an adequate turning radius for trucks coming from Route 116. Provisions for drainage shall be made for the widened section. The road shall be widened within the right-of-way limits as shown on the contract drawings. Upon completion of the project, Libby Road shall be returned to its previous width and drainage.

Upon completion of the project, Libby Street and Savage Street shall have suitable riding surfaces. The road shall be free of excessive or large bumps and cracking in comparison to the condition roads prior to the opening of the detour. If the riding surfaces are not deemed suitable, 12.5 mm overlay shall be placed. Determination of the roads condition and suitability of riding surfaces shall be made by the Resident.

A minimum of 75 mm of PMRAP shall be placed as a wearing surface for the full width of the road on sections of existing gravel roadway. The wearing surface on Libby Road shall remain in this location upon the completion of the project.

A temporary gravel turnaround shall be constructed to the north end of the bridge. The turnaround shall be adequate for a school bus or a plow truck to turn around. The turnaround shall be built within the right of way.

Horizontal Alignment. The access road onto Savage Street, including but not limited to horizontal curve radii and super elevation, shall be designed by a professional engineer, licensed in the State of Maine, for a 30 mph design speed. The minimum width on the access roads shall be 7.2 m.

The access road between Libby Road and the Rest Area shall be designed for trucks turning onto a perpendicular road. The minimum width on the access roads shall be 7.2 m.

The alignment shall be designed to account for the turning characteristics of a WB-20.

Road Surface. The road surface for the access roads shall have a minimum of 75 mm PMRAP for the duration of the project.

Road Base. The access roads shall have a minimum of 600 mm of aggregate subbase course gravel, Type D and E, and shall be designed to support legal loads during the use of the detour.

Maintenance. A smooth traveling surface of the roads shall be maintained throughout the project. If at any point the existing or access roads between Route 116 and the Interstate become unsatisfactory, as determined by the Resident, to travel, the Contractor shall immediately repair the situation.

The contractor shall provide winter maintenance on the Route 116 from the intersection with Powerville Road and the driveway closest to the bridge. Winter maintenance shall include plowing and sand. The road shall be cleared at a minimum to allow the users of the road safe travel to and from work. Additional clearing of the road shall be done at the direction of the Resident.

Portal removal. The timber portal and gate on Libby Road shall be removed and the components shall become the property of the contractor and removed off site.

Privacy fence. Stockade fencing shall be placed along the temporary easement line or the property line of the Nietz and Grant properties adjacent to the detour route. The estimated amounts of fence to be installed are 115' on the Nietz property and 100' on the Grant property. Actual limit of fence installation will be determined by the Resident. The fence shall be a minimum of 5 feet high. The fence shall be placed prior to closing the bridge to traffic. Upon completion of the project, the fences shall be removed and become the property of the contractor. The holes from the fence posts shall be filled and the area returned to its original state at the completion of the project.

510.03 Vehicular and Pedestrian Traffic Not Separated: Paragraph 5, subsection c. the fifth paragraph is revised to read as follows:

c) Approach Road Guardrail. The Special Detour approaches shall have guardrail where side slopes are steeper than three horizontal to one vertical. Approach guardrail shall be attached to the bridge guardrail in a manner that develops the approach guardrail in tension. Approach guardrail shall consist of temporary concrete barriers or an approved equal. The termination of approach guardrail and the end treatment of the rail shall be in accordance with the current AASHTO Roadside Design Guide.

510.09 Basis of Payment This subsection is deleted in its entirety and replaced by the following:

The accepted Special Detour will be paid for at the contract lump sum price. This price shall be full compensation for maintaining traffic by constructing temporary roadways, ramps, and upgrading existing roadways. It shall include all the respective items, as called for in the contract, designed, constructed, maintained, completely removed and the affected areas rehabilitated and stabilized, including loaming, seeding and mulching. Payment shall be considered full compensation for all clearing, guardrail with appropriate end treatments, striping, placement of roadway overlays, placement of PMRAP or equivalent wearing surface, placement and preparation of aggregate subbase, excavation, borrow, installation and removal of stockade fencing, removal of existing portal and gate, restoration of disturbed areas, installation of bus turnaround, striping of access roads, and all incidentals required to construct and remove the detour. Payment for design work associated with the detour shall be paid for under this item. Payment for winter maintenance of Route 116 from the intersection with Powerville Road and the driveway closest to the bridge shall be included under this item.

Payment will be made under:

<u>Pay Item</u>		<u>Pay Unit</u>
510.10	Special Detour Vehicular and Pedestrian Traffic Not Separated	Lump Sum

SPECIAL PROVISION
SECTION 652
MAINTENANCE OF TRAFFIC

Approaches. Approach signing shall include the following signs shown on the Standard Maintenance of Traffic in Construction Zones sheet for "Project Approach Signing - One Way Traffic".

Road Work Ahead
Road Work 500 Feet with 25 MPH Advisory Speed Plate
Road Work 1000 Feet
End Road Work

Work Area. At each work site, signs and channelizing devices as shown on the Standard Maintenance of Traffic in Construction Zones sheets of the plans shall be used as directed by the Resident.

Signs include:

Work Area Ahead
Work Area with 25 MPH Speed Plate
One Lane Road
Flagger Sign
Bridge Closed to Thru Traffic (R11-4)
Bridge Closed (R11-2)
Detour 1000 Feet (W20-2)
Detour Arrow (M4-10) Left & Right
End Detour
Stop Signs
Proceed when clear
Do Not Enter

Other typical signs include:

Trucks Entering
Pavement Ends
Directional Arrows
Bump

All proposed temporary signing shall be shown in the Traffic Control Plan (TCP). Signs, except when tripod mounted, shall be placed on breakaway posts. Signs shall be fluorescence orange type VII background 4' x 8' signs. No type III signs on this project.

The above lists of Approach signs and Work Area signs are representative of the contract requirements. The signs located in the drawings within this specification are required. Other sign legends may also be required.

The road may be closed to traffic during construction. Traffic shall be maintained on temporary one-way access roads connecting the Interstate and Route 116 south of the Penobscot River. An access road shall be designed and constructed to provide entrance only access from Libby Road to the Interstate Rest Area on the northbound side. Another access road shall be designed and constructed to provide exit only access from the southbound lane of the Interstate to Savage Street. The road shall be designed to handle legal Interstate loading. The temporary access roads shall be constructed and maintained within the State's Right of Way.

Detour signing shall be installed prior to bridge closure and maintained through the closure by the Contractor. When the bridge is closed, temporary concrete barriers shall be placed in the interstate crossover immediately south of the Penobscot River to stop traffic from using the crossover to turn around. When the Bridge is open to traffic, the Contractor shall immediately remove all detour signing and close the temporary access roads.

The Contractor will be required to provide a traffic control plan to the Resident within five days after award. The Contractor shall provide a Traffic Control Supervisor, trained in work zone traffic, who will be responsible for providing traffic control management in compliance with the contract, the Manual of Uniform Traffic Control Devices (MUTCD), and contractor's Traffic Control Plan, including supervision of personnel for the installation, inspection, maintenance, and removal of all traffic control devices on the project.

Channelization. Channelization devices shall include the following:

- Type I Barricades
- Type II Barricades
- Type III Barricades
- Drums
- Cones
- Vertical Panel Markers
- Temporary Concrete Barriers
- Flashing Arrow Boards

Channelization devices shall be installed and maintained at all times at the spacing determined by the MUTCD and contractor's Traffic Control Plan, through the work area. Channelization devices consisting of barricades or drums, at a maximum spacing of 15 m [50 feet] shall be used in guardrail areas when neither the existing guardrail nor the new guardrail is in place.

Southbound Interstate Traffic Control. The southbound lanes of the Interstate shall be regulated throughout the use of the detour. All traffic control shall follow the requirements of the MUTCD. All traffic shall be diverted to the left lane before the Exit 244. Through traffic shall remain in the left lane until the rest area. The right lane shall be blocked off until Exit 244 on-ramp. The right lane shall be opened for traffic to access Exit 244. After Exit 244, the right lane shall be designated as a deceleration lane for the temporary exit. After the temporary exit, the interstate's normal function shall resume.

After Exit 244, the centerline of the Interstate shall be painted with a solid white line until after the temporary exit. Through arrows shall be painted in the left lane and right turn arrows painted in the right lane to designate the lane functions. Proper signage and portable-changeable message signs shall be used to inform traffic of detour. Minimum required signage is shown in the drawings within this specification.

Temporary Centerline. A temporary centerline of reflectorized traffic paint shall be marked each day as new pavement to be used by traffic. The temporary centerline shall conform to the standard marking patterns used for permanent markings and will be paid for under Pay Item 510.10. Failure to apply a temporary centerline daily will result in suspension of paving until temporary markings are applied to all previously placed pavement.

652.7 Method of Measurement. This entire Subsection is revised to read:

Work Zone Traffic Control, including Contractor's Traffic Control Plan; Traffic Control Supervisor; furnishing, installation and maintenance of traffic control devices; shall be measured as one lump sum for all work authorized and performed.

652.16 Basis of Payment. This entire Subsection is revised to read:

Work Zone Traffic Control will be paid for at the contract lump sum price. Payment will be full compensation for the Traffic Control Plan; Traffic Control Supervisor; traffic control devices; removing and resetting traffic control devices and maintenance thereof; interstate striping; channelization devices; Interstate striping and pavement markings; and for signs and maintenance thereof.

Maintenance of signs includes: replacing devices damaged, lost, or stolen, and cleaning and moving as many times as necessary throughout the life of the contract, regardless whether the work areas or projects are geographically separated or not separated.

Partial payments for Work Zone Traffic Control will be made as follows:

a) After the Traffic Control Plan is submitted and approved, 15% of the amount bid for Work Zone Traffic Control will be paid.

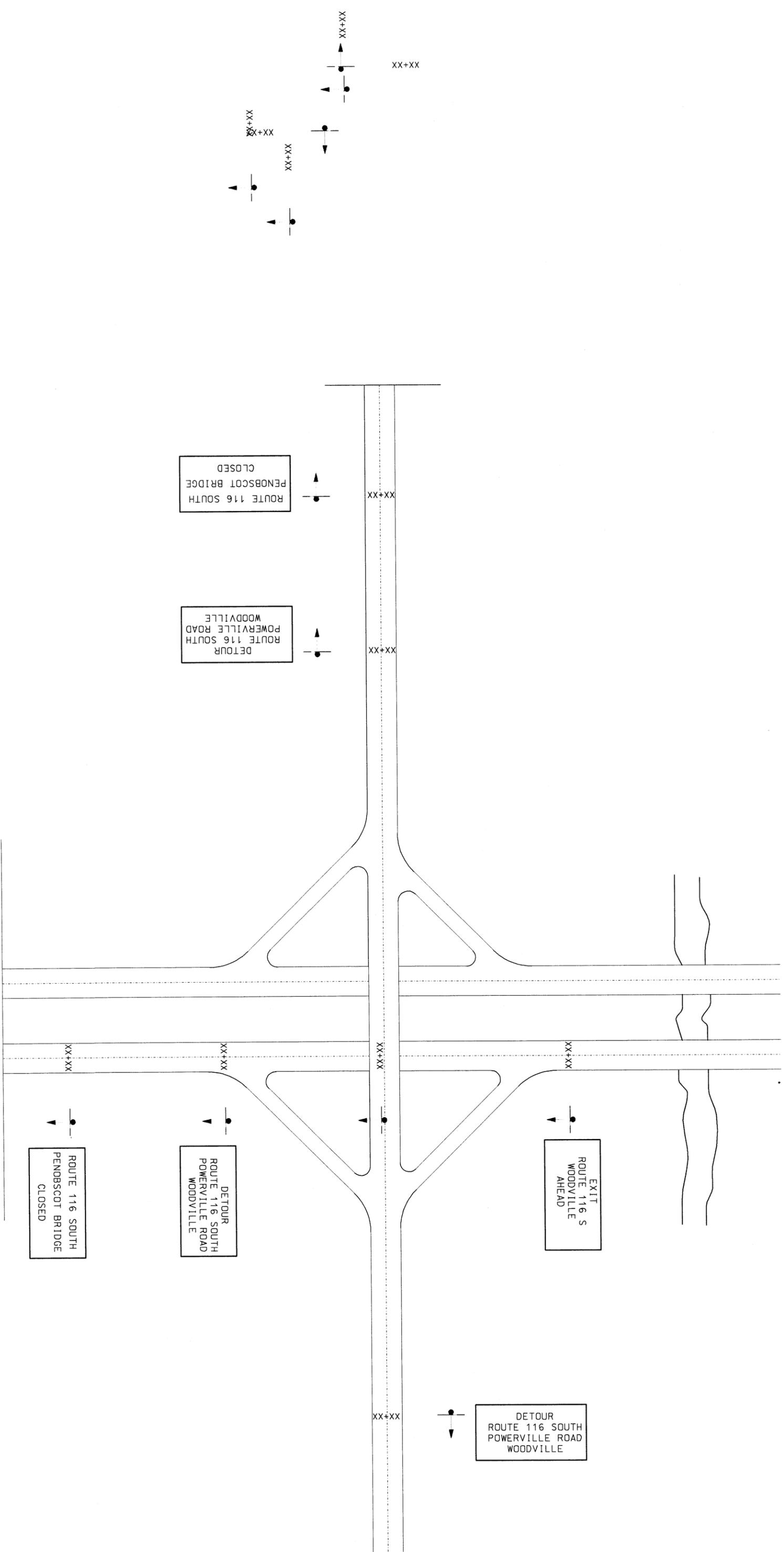
- b) After the completion bridge closure, 60% of the amount bid for Work Zone Traffic Control will be paid.
- c) Upon completion of the entire project, 100% of the amount bid for Work Zone Traffic Control will be paid.

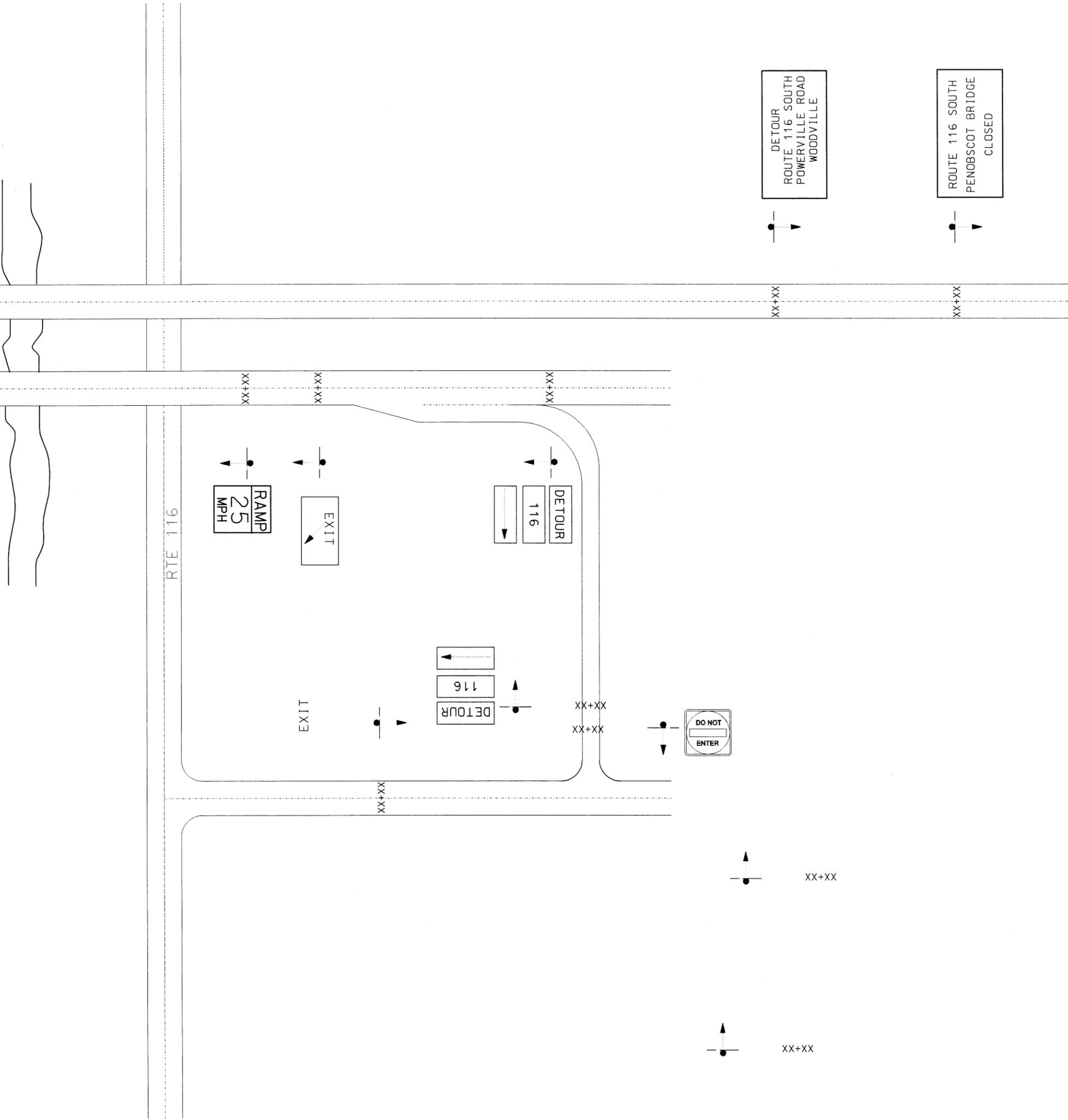
There will be no payment for work done under this pay item after the expiration of contract time.

Payment will be made under:

Failure by the contractor to follow the contracts 652 Special Provisions and/or and or The Manual of Traffic Control Devices (MUTCD) and/or The contractors own Traffic Control Plan will result in a reduction in payment, computed by reducing The Lump Sum Total by 5% per occurrence. The Department's Resident Engineer or any other representative of the Department reserves the right to suspend the work at any time and request a meeting to discuss violations and remedies. The Department shall not be held responsible for any delay in the work do to any suspension under this item.

<u>Pay Item</u>	<u>Pay Unit</u>
652.39 Work Zone Traffic Control	Lump Sum





DETOUR
ROUTE 116 SOUTH
POWERSVILLE ROAD
WOODVILLE

ROUTE 116 SOUTH
PENOBSCOT BRIDGE
CLOSED

RAMP
25
MPH

EXIT

DETOUR
116

DETOUR
116

DO NOT
ENTER

EXIT

XX+XX

XX+XX

RTE 116